



Interoffice Memo Office of Design Policy & Support

DATE: 3/29/2022

FILE: P.I.# 0015632
Coffee County / GDOT District 4 - Tifton
Bridge Replacement CR 705/Bridgetown Road @ Satilla River 11 MI W of
Douglas

FROM: *Dane Peters*
for R. Christopher Rudd, PE, State Design Policy Engineer

TO: SEE DISTRIBUTION

SUBJECT: APPROVED CONCEPT REPORT

Attached is the approved Concept Report for the above subject project.

Attachment

Distribution:

Hiral Patel, Director of Engineering
Joe Carpenter, Director of P3
Albert Shelby, Director of Program Delivery
Clement Solomon, Director, Division of Intermodal
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Erik Rohde, State Project Review Engineer
Patrick Allen, State Materials Engineer
Nick Fields, State Utilities Administrator
Benny Walden, Statewide Location Bureau Chief
Scott Chambers, District Engineer
Jason Willingham, District Preconstruction Engineer
Shane Pridgen, District Utilities Manager
Raquel McMillan, Project Manager
BOARD MEMBER - 12th Congressional District




Limited Scope Project Concept Report

Project Type:	Bridge Replacement	P.I. Number:	0015632
GDOT District:	4	County:	Coffee
Federal Route Number:	N/A	State Route Number:	N/A
Project Number:	N/A		

This project proposes to replace the 2-lane bridge on CR705/Bridgetown Rd over Satilla River, West of Douglas, with a new 2-lane bridge. The roadway approaches on both sides of the bridge will be improved to conform to GDOT standards.

*** Concept Report update received 2/8/2021**

Submitted for approval:

	12-23-21
Steven Gaines, P.E., American Consulting Professionals	Date
	1-19-2022
State Program Delivery Administrator	Date
	1-12-22
GDOT Project Manager	Date

Recommendation for approval: *** Recommendations are on file - ING**

* Eric Duff	1/20/2022
State Environmental Administrator	Date
* Oladimeji Onabanjo	1/21/2022
State Traffic Engineer	Date
* Donn Digamon	1/22/2022
State Bridge Engineer	Date
* Scott Chambers	1/20/2022
District Engineer	Date

for

- ☐ MPO Area: This project is consistent with the MPO adopted Regional Transportation Plan (RTP)/Long Range Transportation Plan (LRTP).
- ☒ Rural Area: This project is consistent with the goals outlined in the Statewide Transportation Plan (SWTP) and/or is included in the State Transportation Improvement Program (STIP).

* Matt Markham	2/2/2022
State Planning Deputy Director	Date

Approval: 

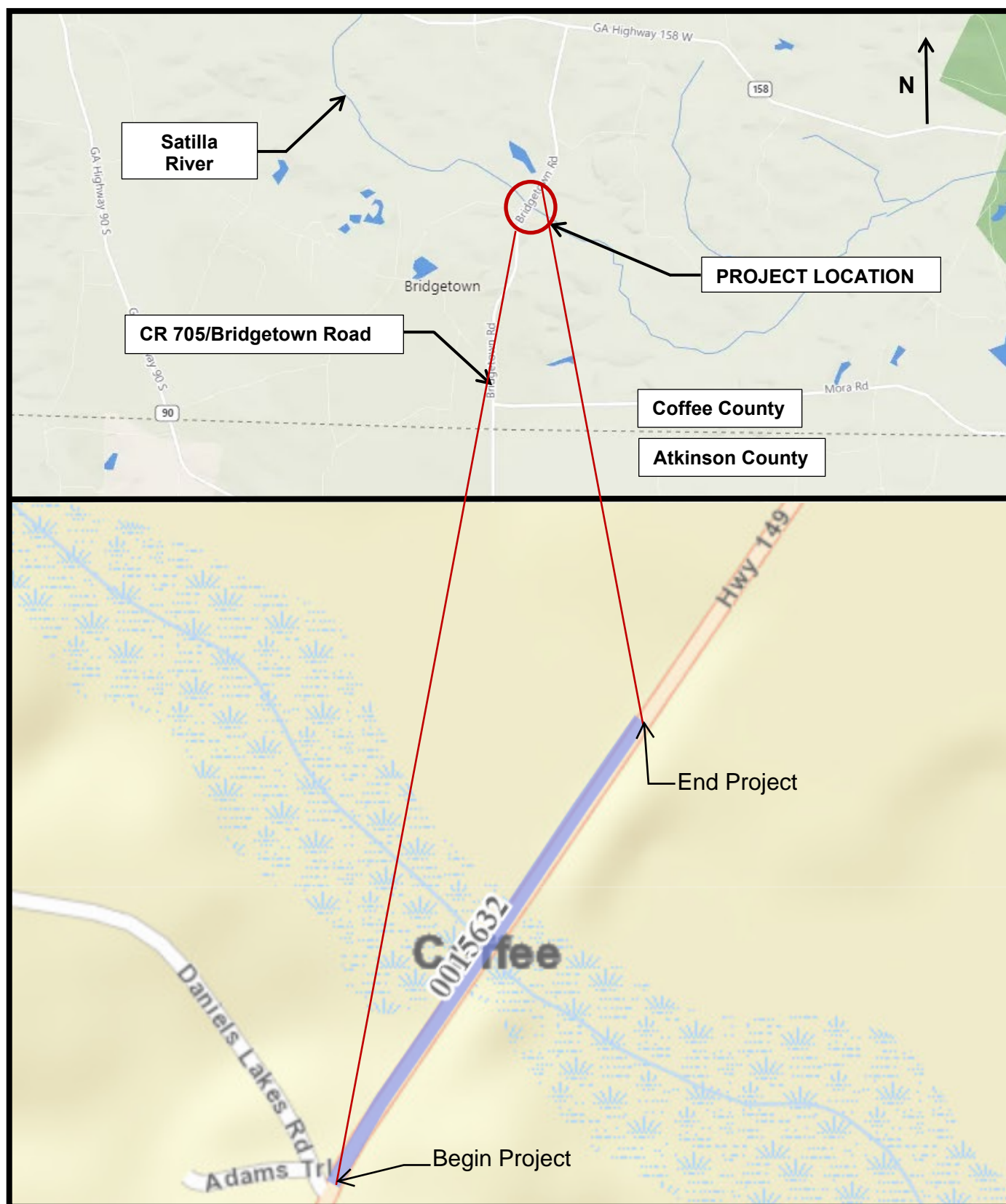
Concur:	3/28/2022
GDOT Director of Engineering	Date

Approve:	3/29/22
GDOT Chief Engineer	Date

*** Recommendations were also received from the following: - ING**

- * Office of Engineering Services: Erik Rohde (1/24/2022)**
- * Office of Utilities: Marcela Coll (2/1/2022)**
- * Office of Intermodal: Alan C. Hood (2/1/2022)**
- * District 4 Utility Manager : Shane Pridgen (1/20/2022)**
- * District 4 Preconstruction Engineer: Jason Willingham (2/3/2022)**

PROJECT LOCATION MAP



PLANNING & BACKGROUND DATA

Prepared By: Office of Bridge Design **Date:** 1/13/2020

Project Justification Statement: The bridge on CR 705 (Bridgetown Road) over Satilla River, Structure ID 069-0035-0 was built in 1960. The bridge consists of 10 reinforced concrete deck girder spans on concrete caps with partially encased H-Piles. The bridge was designed for an H-15 vehicle, which is below the current standard. The overall condition of the bridge was classified as poor. The deck condition is classified as satisfactory, with cracking present throughout the deck, approach slabs, curbs, and handrails. Minor pop-outs and moderate abrasion are also present on the deck. A spall with exposed rebar is present on the bottom of the deck in span 1. The superstructure condition is classified as good, with minor flexure cracking present in all T-beams. The substructure condition is classified as poor. There are minor spalls and cracking in the caps. The encasements are in poor condition. Underwater inspection found most piles to have section loss. The section loss ranges from partial (starting at the web and expanding to the flange edges) to loss covering the entire section leaving approximately 3/16 inches of remaining steel. The foundation elevations are also unknown, which causes the bridge to be classified as scour critical. The bridge has been posted to a limit of 18 tons for H-Modified trucks. Due to the age of the structure, design not meeting current standards, load posting, being classified as scour critical and poor condition of the substructure, replacement of this bridge is recommended.

Existing conditions: The existing 300' length bridge is composed of concrete and steel and has a deck width of 32.3' with two 11' lanes. CR 705/Bridgetown Road is a two 11' lane rural major collector with wooded areas to each side. The existing bridge is located approximately eleven miles from the city center of Douglas.

Other projects in the area: 0016241 (Hwy 149/Youngie Fussell Road/Bridgetown Road Widen and Resurface)
Management Let Date is 7/22/22

MPO: N/A - not in an MPO **TIP #:** N/A
Congressional District(s): 12

Federal Oversight: ☐ PoDI ☒ Exempt ☐ State Funded ☐ Other

Projected Traffic: 24 HR T: 12.0 % Current Year (2021): 500
Open Year (2027): 500 Design Year (2047): 575
Traffic Projections Performed by: Atlas Technical Consultants, Inc.
Date approved by the GDOT Office of Planning: 1/7/22

AASHTO Functional Classification (Mainline): Major Collector

AASHTO Context Classification (Mainline): Rural

AASHTO Project Type (Mainline): Construction on Existing Roads

Is the project located on a NHS roadway? ☒ No ☐ Yes

Complete Streets - Bicycle, Pedestrian, and/or Transit Standards Warrants:

Warrants met: ☒ None ☐ Bicycle ☐ Pedestrian ☐ Transit

Is this a 3R (Resurfacing, Restoration, & Rehabilitation) Project? ☒ No ☐ Yes

Pavement Evaluation and Recommendations

Initial Pavement Evaluation Summary Report Required? ☒ No ☐ Yes
Feasible Pavement Alternatives: ☒ HMA ☐ PCC ☐ HMA & PCC

Is the project located on a Special Roadway or Network? ☒ No ☐ Yes

Do the limits of the project include one or more signalized intersections? ☒ No ☐ Yes

Is Federal Aviation Administration coordination anticipated? ☒ No ☐ Yes

Any construction equipment in excess of 200 feet above the roadway elevation must be evaluated by the FAA. Evaluation by filing of "Notice of Proposed Construction" FAA form 7460-1 must be accomplished not earlier than 18 months and not later than 120 days prior to construction.

DESIGN AND STRUCTURAL

Description of the proposed project: The proposed 360-foot length bridge will consist of one 11' lane in each direction with 4' shoulders. The proposed project length is 1600 feet. The profile grade of the proposed bridge will be raised approximately 6 feet higher than the existing bridge because of increased structure depth and bridge hydraulic requirements. The bridge approaches will consist of one 11' lane in each direction and 4' shoulders, 4' of which is paved. The proposed bridge will be constructed on the existing alignment while traffic is maintained on an on-site detour alignment and detour bridge.

Major Structures:

Structure	Existing	Proposed
069-0035-0	300' length; 32.3' deck width; two 10.5' lanes; 2' shoulders; 10 spans; steel and concrete	360' length; 33.08' deck width; two 11' lanes; 4' shoulders; 3 spans; concrete

Accelerated Bridge Construction (ABC) techniques anticipated: ☒ No ☐ Yes

Multiple techniques are applicable to this project, but it is unlikely that they will be utilized due to the fact that an on-site detour will allow for traffic to utilize the road during construction.

Prefabricated Bridge Elements and Systems (PBES): Prefabricated elements could be utilized to facilitate faster construction. However, given that on-site detour will allow for traffic to utilize the road during construction, it is unlikely these elements will be used.

Mainline Design Features:

CR 705/Bridgetown Rd	Functional Classification: <i>Major Collector</i>		
Feature	Existing	*Policy	Proposed
Typical Section:			
- Number of Through Lanes	2		2
- Lane Width(s) (-ft)	11-ft	11-ft	11-ft
- Median Width (-ft) & Type	N/A	N/A	N/A
- Shoulder Width (-ft) (Outside)	2 ft	4-ft; 4-ft paved	4-ft; 4-ft paved
- Cross Slope (%)	2%	2%	2%
- Outside Shoulder Slope (%)	6%	6%	6%
- Sidewalks (-ft)	None	None	None
- Auxiliary Lanes (# LTL, RTL or TWLTL / -ft width)	None		None
- Bike Accommodations	None	None	None
Posted Speed (mph)	Unknown		55 mph
Design Speed (mph)	Unknown	55 mph	55 mph
Minimum Horizontal Curve Radius (-ft)	Unknown	1060-ft	1930-ft
Maximum Superelevation Rate (%)	Unknown	6%	6% *
Maximum Grade (%)	Unknown	6%	3.8%
Access Control	None	None	None
Design Vehicle	Unknown		SU
Pavement Type	HMA		HMA

*According to current GDOT Design Policy if applicable

Design Exceptions/Design Variances to FHWA or GDOT Controlling Criteria anticipated: N/A

Design Variances to GDOT Standard Criteria anticipated: N/A

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Lighting Required: ☒ No ☐ Yes

Off-site Detours Anticipated: ☒ No ☐ Undetermined ☐ Yes

If yes: Roadway type to be closed: ☐ Local Road ☐ State Route

Detour Route selected: ☐ Local Road ☐ State Route

District Concurrence w/Detour Route: ☐ No/Pending ☐ Received

Transportation Management Plan [TMP] Required: ☐ No ☒ Yes

If Yes: Project classified as: ☒ Non-Significant

TMP Components Anticipated: ☒ TTC

INTERCHANGES AND INTERSECTIONS

Interchanges/Major Intersections: N/A

Intersection Control Evaluation (ICE) Required: ☒ No ☐ Yes

Roundabout Concept Validation Required: ☒ No ☐ Yes ☐ Completed

UTILITY AND PROPERTY

Railroad Involvement: None

Utility Involvements: Satilla EMC, Windstream

SUE Required: ☒ No ☐ Yes

Public Interest Determination Policy and Procedure recommended: ☒ No ☐ Yes

Right-of-Way (ROW): Existing width: 80ft. Proposed width: 140ft.

Required Right-of-Way anticipated: ☐ None ☒ Yes ☐ Undetermined

Easements anticipated: ☐ None ☒ Temporary ☐ Permanent * ☐ Utility ☐ Other

** Permanent easements include the right to place utilities.*

Anticipated total number of impacted parcels:		5
Displacements anticipated:	Businesses:	0
	Residences:	0
	Other:	0
Total Displacements:		0

Location and Design approval: ☐ Not Required ☒ Required

Impacts to USACE property anticipated: ☒ No ☐ Yes ☐ Undetermined

ENVIRONMENTAL AND PERMITS

Anticipated Environmental Document: *NEPA ~ CE*

Level of Environmental Analysis:

- ☒ The environmental considerations noted below are based on preliminary desktop or screening level environmental analysis and are subject to revision after the completion of resource identification, delineation, and agency concurrence.
- ☐ The environmental considerations noted below are based on the completion of resource identification, delineation, and agency concurrence.

MS4 Permit Compliance – Is the project located in a MS4 area?

☒ No ☐ Yes

If yes, is the GDOT MS4 Permit anticipated to apply to all or part of this project?

☒ No ☐ Yes

Is Non-MS4 water quality mitigation anticipated?

☒ No ☐ Yes

Environmental Permits, Variances, Commitments, and Coordination anticipated: A National Pollutant Discharge Elimination System (NPDES) permit is anticipated for projects disturbing more than one acre. Additional potential permitting is described below.

Is a PAR required?

☒ No ☐ Yes ☐ Completed

Air Quality:

Is the project located in an Ozone Non-attainment area?

☒ No ☐ Yes

Is a Carbon Monoxide hotspot analysis required?

☒ No ☐ Yes

NEPA/GEPA Comments & Information: The level of environmental documentation required would be determined prior to construction based on the extent of environmental impacts and funding source. Ecology, history, and archaeology have conducted field surveys. The anticipated level of NEPA documentation is a Categorical Exclusion (CE).

Ecology: A list of state and federally protected species was obtained using the U.S. Fish and Wildlife Service's (USFWS) Information for Planning and Consulting (IPaC) and the Georgia DNR Rare Natural Elements lists. State protected species that have known occurrences within 3 miles of the project area or within the HUC 10 watershed are considered in the report (using the Georgia's Natural, Archaeological, and Historic Resources Geographic Information System [GNAHRGIS] Ecology module). The following species are listed as potentially occurring within the project vicinity: mimic glass lizard (*Ophisaurus mimicus*), blackbanded sunfish (*Enneacanthus chaetodon*), and Suwanee alligator snapping turtle (*Macrochelys suwanniensis*). Two perennial streams, one wetland, and one open water were noted within the project vicinity from desktop review of National Wetland Inventory (NWI) and National Hydrography Dataset (NHD) maps and field survey. The necessity of a Section 404 Permit and a buffer variance from the Georgia Department of Natural Resources- Environmental Protection Division (EPD) would be determined later.

History: Field and desktop reviews for historic resources, comprised of buildings, districts, structures, sites, and objects constructed on or before 1969, were conducted within an environmental survey boundary (ESB) and corresponding viewshed from the furthest extent of the ESB. This review included the National Register of Historic Places (NRHP), National Historic Landmarks (NHL), Georgia's Natural, Archaeological, and Historic Resources GIS (GNAHRGIS), Irwin County tax assessor's record, U.S. Geological Survey (USGS) topographic maps, Nationwide Environmental Title Research (NETR) aerial photography, Google Street View, and Georgia's Historical Marker program. As a result of these identification efforts, two (2) resources were identified within the proposed project's ESB and require field assessment and evaluation for National Register eligibility as outlined in 36 CFR Part 60.4 of the National Historic Preservation Act. Based on the desktop screening only, one (1) resource has been determined to be potentially eligible for listing in the NRHP. The eligibility determinations are preliminary and are

subject to change during the environmental assessment and review process. State Historic Preservation Officer (SHPO) concurrence is required.

Archeology: The Phase I survey resulted in the identification of two newly recorded archaeological sites. Coordination with the SHPO is required for this project.

Noise Effects: Noise analysis is not anticipated as the project is not anticipated to accommodate an increase in traffic and the project does not substantially shift the roadway toward noise sensitive receptors.

Public Involvement: A Public Involvement Open House (PIOH) may be required for this project after the completion of Resource Identification.

Major stakeholders: Coffee County

COORDINATION, ACTIVITIES, RESPONSIBILITIES, AND COSTS

Constructability/Construction: No constructability issues noted. The road will remain open during construction and an on-site detour with detour bridge will be installed.

Project Meetings: Concept Team Meeting (11/3/21)

Other coordination to date: Detour letters have been sent to Local Government, EMS and Board of Education. Coffee County EMS expressed major concerns because of potential impacts to the Bridgetown Volunteer Fire Department, which is located approximately one mile south of the project.

Project Activity	Party Responsible for Performing Task(s)
Concept Development	Consultant - American Consulting Professionals, LLC
Design	Consultant - American Consulting Professionals, LLC
Right-of-Way Acquisition	GDOT (Right-of-Way)
Utility Coordination (Preconstruction)	GDOT (Utilities)
Utility Relocation (Construction)	Utility Owners
Letting to Contract	GDOT (Bidding Administration)
Construction Supervision	GDOT (Construction)
Providing Material Pits	Contractor
Providing Detours	Contractor
Environmental Studies, Documents, & Permits	GDOT (Office of Environmental Services)
Environmental Mitigation	GDOT (Office of Environmental Services)
Construction Inspection & Materials Testing	GDOT (Construction)

Project Cost Estimate Summary and Funding Responsibilities:						
	PE Activities		ROW	Reimbursable Utilities	CST*	Total Cost
	PE Funding	Section 404 Mitigation				
Date of Estimate:	7/31/20	10/8/21	1/4/22	8/4/21	2/21/22	
Funded By:	Federal/State	Federal/State	Federal/State/Local	Federal/State	Federal/State	
Programmed Cost:	\$905,000		\$250,000	\$30,000	\$2,700,000	\$3,885,000
Estimated Cost:	\$905,000	\$104,200	\$127,000	\$0	\$4,461,866	\$5,598,066
Total Cost Difference:						\$1,713,066**

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*CST Cost includes: Construction, Engineering and Inspection, Contingencies and Liquid AC Cost Adjustment.

**Anticipated source of additional required funding is federal funding

ALTERNATIVES DISCUSSION

Alternative selection:

Preferred Alternative: Replace existing bridge on the existing alignment and utilize an on-site detour.			
Estimated Property Impacts:	5 Parcels	Estimated Total Cost:	\$5,598,066
Estimated ROW Cost:	\$127,000	Estimated CST Time:	15 Months
Rationale: This alternative would replace the existing bridge in-place and utilize an on-site detour on the east side of the road. Roadway approach improvements would be required on both sides of the bridge to accommodate raising the profile grade of the proposed bridge approximately 6 feet to accommodate bridge hydraulics requirements and increased structure depth. A temporary on-site detour would be installed on the east side (downstream) of the bridge to facilitate traffic flow while the existing bridge is removed and the new bridge is installed. This alternative was chosen because it will have a smaller footprint, requiring less right of way acquisition and decreasing environmental impacts when compared with the permanent offset alignment alternative.			

Alternative 1: Replace the bridge on a permanent offset alignment and maintain traffic on the existing alignment.			
Estimated Property Impacts:	7 Parcels	Estimated Total Cost:	\$5,720,000
Estimated ROW Cost*:	\$240,000	Estimated CST Time:	15 Months
Rationale: This alternative would replace the existing bridge to the west on a parallel alignment and maintain traffic on the existing alignment. Improvements to the bridge approaches would require two additional horizontal curves and 1300 lf of additional roadway when compared with the preferred alternative. Additional topographic survey and environmental surveys would be required. The required length of the proposed bridge would be approximately 50 lf longer than the preferred alternative due to the characteristics of the stream channel. No offsite or on-site detours would be required for this alternative. This alternative is not preferred because it would create a larger footprint, increase environmental impacts, and increase right of way acquisition costs.			

*Estimated ROW cost by design team.

Alternative 2: Replace existing bridge on the existing alignment and utilize an off-site detour.			
Estimated Property Impacts:	5 Parcels	Estimated Total Cost:	\$4,500,000
Estimated ROW Cost*:	\$110,000	Estimated CST Time:	12 Months
Rationale: This alternative would replace the existing bridge in-place and utilize an offsite detour. The detour would require vehicles to travel an additional 6.4 miles on local roads and state routes with equal or greater functional capacity, including SR 90 and SR 158. Detour Coordination Letters were submitted to Local Government, EMS and County Schools. Coffee County EMS expressed major concerns because of potential impacts to the Bridgetown Volunteer Fire Department, which is located approximately one mile south of the project. The major concerns included lack of local roads in adequate condition for reaching nearby service areas, increased travel time for volunteer fire personnel reaching the station and increased travel time for fire trucks to reach service areas. This alternative was not chosen because of impact to emergency response times although the construction cost, right-of-way impacts and environmental impacts are less than the preferred alternative.			

**Estimated ROW cost by design team.*

No-Build Alternative: Retain the existing bridge.			
Estimated Property Impacts:	None	Estimated Total Cost:	\$0
Estimated ROW Cost:	\$0	Estimated CST Time:	0 Months
Rationale: This alternative is not preferred because it does not address project need and purpose.			

Comments:

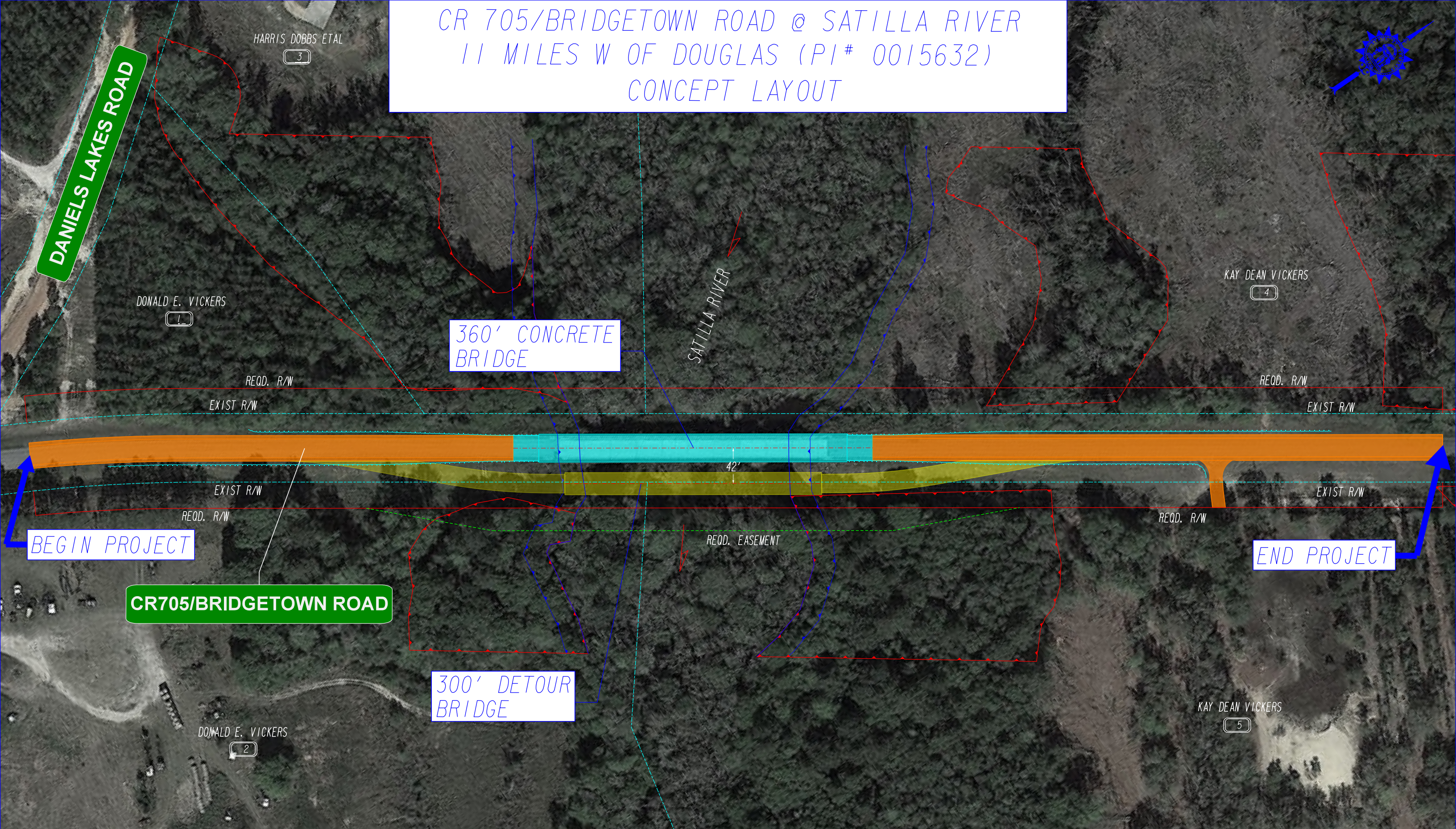
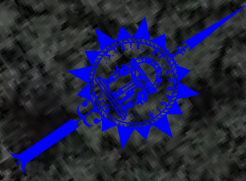
LIST OF ATTACHMENTS/SUPPORTING DATA

1. Concept Layout and Typical Sections
2. Detailed Cost Estimates:
 - a. Construction Estimate including Engineering and Inspection and Contingencies
 - b. Revisions to Programmed Costs forms, & Liquid AC Cost Adjustment forms
 - c. Right-of-Way
 - d. Environmental Mitigation
 - e. Utilities
3. Concept Utility Report
4. Traffic Projections Memorandum
5. S I & A Report(s)
6. MS4 Concept Report Summary
7. Minutes – Concept Team Meeting

Attachment 1

Concept Layout and Typical Sections

CR 705/BRIDGETOWN ROAD @ SATILLA RIVER
11 MILES W OF DOUGLAS (PI# 0015632)
CONCEPT LAYOUT



BEGIN PROJECT

END PROJECT

CR705/BRIDGETOWN ROAD

360' CONCRETE
BRIDGE

300' DETOUR
BRIDGE

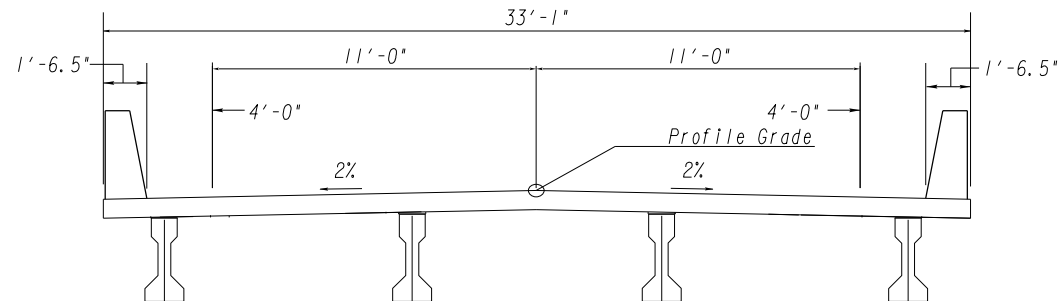
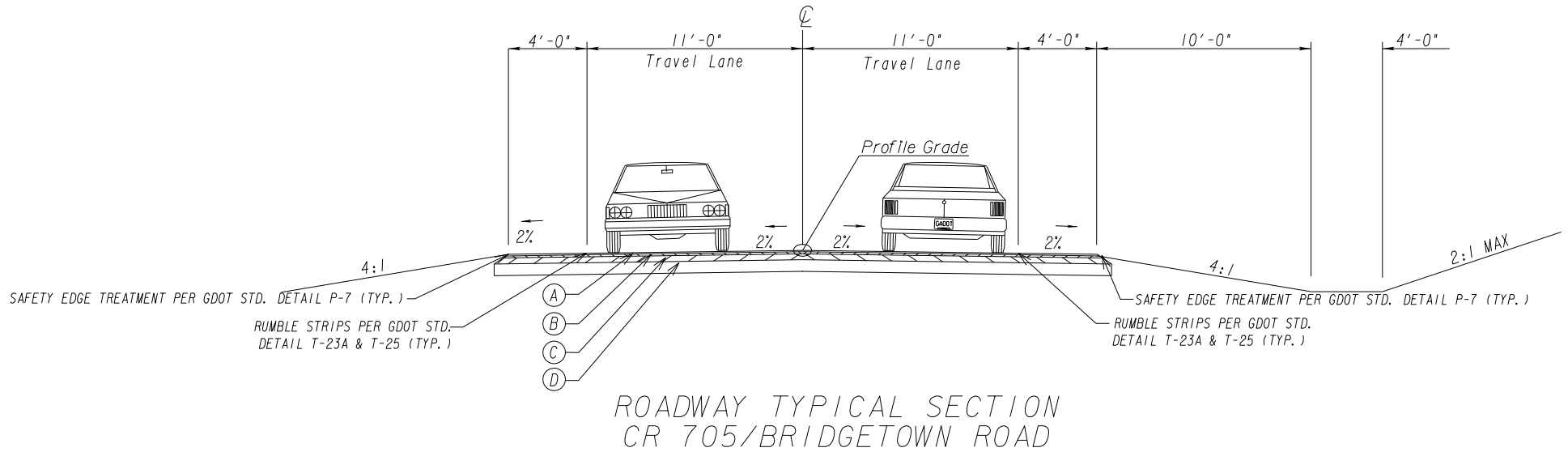
GDOT Georgia Department of Transportation

American American Consulting Professionals

2047 Gees Mill Road, Suite 211
Conyers, Georgia 30013
Phone: (478) 207-0635

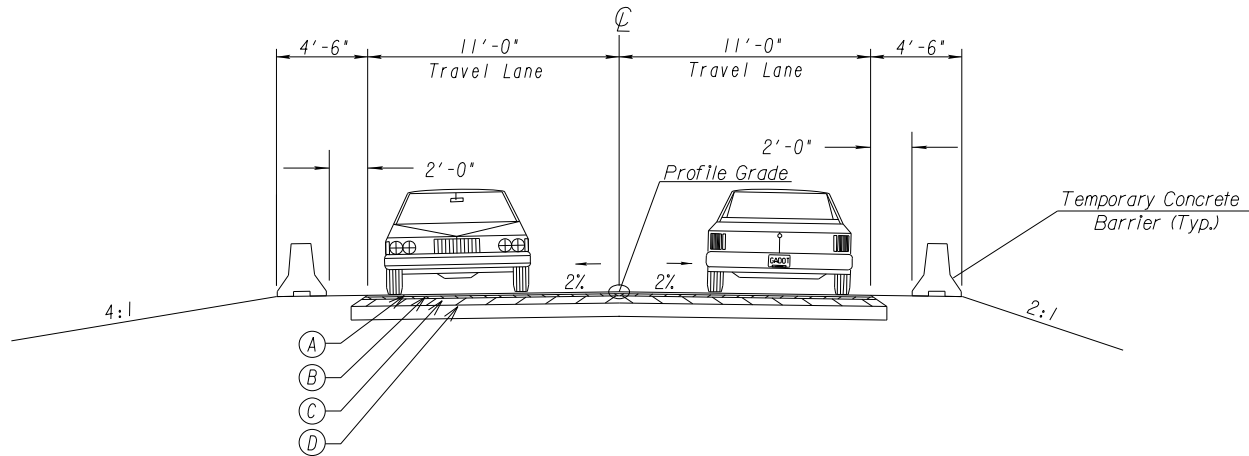
LEGEND	
	PROPOSED ROADWAY
	PROPOSED BRIDGE/ APPROACH SLAB
	PROPOSED GUARDRAIL
	STREAM BANK
	CONSTRUCTION EASEMENT
	PROPERTY LINE
	REQ'D R/W
	ESA BOUNDARY (WETLAND/ OPEN WATER)
	ESA BOUNDARY (STREAM BUFFER)



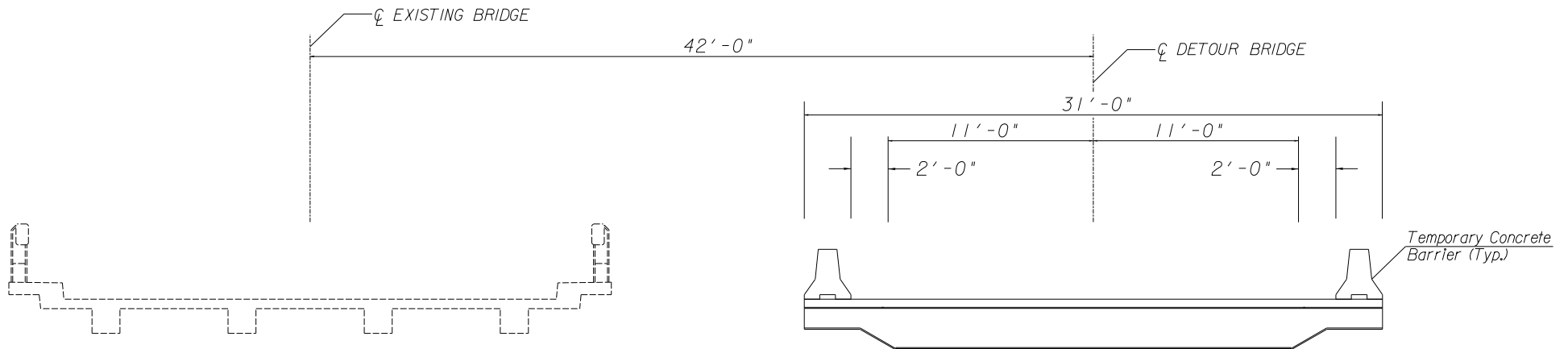


PAVEMENT SECTION

- Ⓐ 9.5 MM RECYCLED AC SUPERPAVE, TP 11, GP 2 ONLY, INCL. BITUM. MATL AND H. LIME - 135*/SY
- Ⓑ 19 MM RECYCLED AC SUPERPAVE, GP 1 OR 2, INCL. BITUM. MATL AND H. LIME - 220*/SY
- Ⓒ 25 MM RECYCLED AC SUPERPAVE, GP 1 OR 2, INCL. BITUM. MATL AND H. LIME - 330*/SY
- Ⓓ GRADED AGGREGATE BASE - 8.0"



DETOUR TYPICAL SECTION
CR 705/BRIDGETOWN ROAD



DETOUR BRIDGE TYPICAL SECTION
CR 705/BRIDGETOWN ROAD

PAVEMENT SECTION

- Ⓐ 9.5 MM RECYCLED AC SUPERPAVE, TP 11, GP 2 ONLY, INCL. BITUM. MATL AND H. LIME - 135*/SY
- Ⓑ 19 MM RECYCLED AC SUPERPAVE, GP 1 OR 2, INCL. BITUM. MATL AND H. LIME - 220*/SY
- Ⓒ 25 MM RECYCLED AC SUPERPAVE, GP 1 OR 2, INCL. BITUM. MATL AND H. LIME - 330*/SY
- Ⓓ GRADED AGGREGATE BASE - 6.0"

Attachment 2

Detailed Cost Estimates

Project Cost Estimate	
Concept Name: 0015632_2-11-22 Cost Estimate Name: 0015632_2-11-22	

Projects Cost Estimate	
Processed on: Feb-22-2022 08:42 AM	
<p>CONCEPT NAME: 0015632_2-11-22 COST ESTIMATE NAME: 0015632_2-11-22</p> <p>SPEC YEAR: 21</p> <p>ITEM HISTORY: BHP-ALL - Statewide - 24 months</p> <p>DESCRIPTION: Replace the structurally deficient 2-lane bridge on CR 705/Bridgetown Road over Satilla River West of Douglas w</p> <p>ESTIMATE PHASE: 2-DE - Designers Estimate</p>	
<p align="center">ITEMS FOR CONCEPT NAME 0015632_2-11-22</p>	

0100 - Roadway						
Line Number	Item	Quantity	Units	Price	Description	Amount
5	150-1000	1.00	LS	\$110,000.00	TRAFFIC CONTROL - 0015632	\$110,000.00
240	150-5010	4.00	EA	\$8,735.10	TRAFFIC CONTROL, PORTABLE IMPACT ATTENUATOR	\$34,940.39
10	210-0100	1.00	LS	\$300,000.00	GRADING COMPLETE - 0015632	\$300,000.00
160	433-1200	214.00	SY	\$213.46	REINF CONC APPROACH SLAB, INCL SLOPED EDGE	\$45,680.45
230	620-0100	1000.00	LF	\$42.25	TEMPORARY BARRIER, METHOD NO. 1	\$42,246.34
210	634-1200	8.00	EA	\$176.93	RIGHT OF WAY MARKERS	\$1,415.40
140	641-1100	84.00	LF	\$109.62	GUARDRAIL, TP T	\$9,207.84
35	641-1200	1679.00	LF	\$27.64	GUARDRAIL, TP W	\$46,413.89
40	641-5001	2.00	EA	\$1,655.31	GUARDRAIL ANCHORAGE, TP 1	\$3,310.62
45	641-5015	2.00	EA	\$3,079.31	GUARDRAIL TERMINAL, TP 12A, 31 IN, TANGENT, ENERGY-ABSORBING	\$6,158.63
Roadway Total						\$599,373.56
0110 - Pavement						
Line Number	Item	Quantity	Units	Price	Description	Amount
15	310-1101	2214.00	TN	\$41.64	GR AGGR BASE CRS, INCL MATL	\$92,199.84
185	318-3000	90.00	TN	\$54.22	AGGR SURF CRS	\$4,879.41
220	402-3102	403.00	TN	\$133.16	RECYCLED ASPH CONC 9.5 MM SUPERPAVE, TYPE II, BLEND 1, INCL BITUM MATL & H LIME	\$53,664.54
20	402-3121	787.00	TN	\$107.18	RECYCLED ASPH CONC 25 MM SUPERPAVE, GP 1 OR 2, INCL BITUM MATL & H LIME	\$84,350.08
25	402-3190	525.00	TN	\$111.45	RECYCLED ASPH CONC 19 MM SUPERPAVE, GP 1 OR 2,INCL BITUM MATL & H LIME	\$58,513.42
30	413-0750	638.00	GL	\$1.87	TACK COAT	\$1,194.58
225	456-2015	1.00	GLM	\$1,556.72	INDENTATION RUMBLE STRIPS - GROUND-IN-PLACE (SKIP)	\$1,556.72
Pavement Total						\$296,358.59
0200 - Drainage						
Line Number	Item	Quantity	Units	Price	Description	Amount
190	441-0301	4.00	EA	\$2,979.14	CONC SPILLWAY, TP 1	\$11,916.58
195	550-2240	30.00	LF	\$61.06	SIDE DRAIN PIPE, 24 IN, H 1-10	\$1,831.86
200	550-3424	2.00	EA	\$868.98	SAFETY END SECTION 24 IN, SIDE DRAIN, 4:1 SLOPE	\$1,737.96
205	576-1015	160.00	LF	\$48.31	SLOPE DRAIN PIPE, 15 IN	\$7,729.20
Drainage Total						\$23,215.60
0300 - Temporary Erosion Control						
Line Number	Item	Quantity	Units	Price	Description	Amount
75	163-0232	2.00	AC	\$723.00	TEMPORARY GRASSING	\$1,446.00
80	163-0240	43.00	TN	\$132.82	MULCH	\$5,711.32
85	163-0301	2.00	EA	\$750.00	CONSTRUCT AND REMOVE CONSTRUCTION EXITS	\$1,500.00
245	163-0528	1000.00	LF	\$7.89	CONSTRUCT AND REMOVE FABRIC CHECK DAM - TYPE C SILT FENCE	\$7,891.81
90	165-0030	1750.00	LF	\$0.50	MAINTENANCE OF TEMPORARY SILT FENCE, TP C	\$875.93
250	165-0041	500.00	LF	\$3.00	MAINTENANCE OF CHECK DAMS - ALL TYPES	\$1,500.00
95	165-0101	2.00	EA	\$629.69	MAINTENANCE OF CONSTRUCTION EXIT	\$1,259.37
175	165-0310	1.00	EA	\$400.00	MAINTENANCE OF CONSTRUCTION EXIT TIRE WASH AREA (PER EACH)	\$400.00
100	167-1000	2.00	EA	\$443.96	WATER QUALITY MONITORING AND SAMPLING	\$887.93
105	167-1500	15.00	MO	\$622.45	WATER QUALITY INSPECTIONS	\$9,336.69
110	171-0030	3500.00	LF	\$4.50	TEMPORARY SILT FENCE, TYPE C	\$15,740.44
115	643-8200	1250.00	LF	\$2.79	BARRIER FENCE (ORANGE), 4 FT	\$3,493.16
Temporary Erosion Control Total						\$50,042.65

0400 - Permanent Erosion Control						
Line Number	Item	Quantity	Units	Price	Description	Amount
50	700-6910	3.00	AC	\$1,243.79	PERMANENT GRASSING	\$3,731.36
55	700-7000	9.00	TN	\$35.49	AGRICULTURAL LIME	\$319.45
60	700-8000	3.00	TN	\$937.05	FERTILIZER MIXED GRADE	\$2,811.14
65	700-8100	150.00	LB	\$4.04	FERTILIZER NITROGEN CONTENT	\$605.31
70	716-2000	6000.00	SY	\$2.11	EROSION CONTROL MATS, SLOPES	\$12,653.04
Permanent Erosion Control Total						\$20,120.30
0600 - Signing						
Line Number	Item	Quantity	Units	Price	Description	Amount
180	636-1033	27.00	SF	\$23.39	HIGHWAY SIGNS, TP 1 MATL, REFL SHEETING, TP 9	\$631.57
120	636-1036	33.00	SF	\$27.36	HIGHWAY SIGNS, TP 1 MATL, REFL SHEETING, TP 11	\$902.95
145	636-2070	182.00	LF	\$10.26	GALV STEEL POSTS, TP 7	\$1,866.57
Signing Total						\$3,401.09
0610 - Pavement Marking						
Line Number	Item	Quantity	Units	Price	Description	Amount
125	653-1501	2500.00	LF	\$0.87	THERMOPLASTIC SOLID TRAF STRIPE, 5 IN, WHITE	\$2,179.12
130	653-1502	2000.00	LF	\$0.90	THERMOPLASTIC SOLID TRAF STRIPE, 5 IN, YELLOW	\$1,804.18
235	653-3502	500.00	GLF	\$0.77	THERMOPLASTIC SKIP TRAF STRIPE, 5 IN, YELLOW	\$382.86
135	654-1001	120.00	EA	\$6.33	RAISED PVTM MARKERS TP 1	\$759.53
150	657-1085	720.00	LF	\$8.41	PREFORMED PLASTIC SOLID PVTM MKG, 8 IN, CONTRAST (BLACK-WHITE), TP PB	\$6,054.11
155	657-6085	720.00	LF	\$8.88	PREFORMED PLASTIC SOLID PVTM MKG, 8 IN, CONTRAST (BLACK-YELLOW), TP PB	\$6,390.06
Pavement Marking Total						\$17,569.86
0801 - Bridge 1						
Line Number	Item	Quantity	Units	Price	Description	Amount
165	540-1101	1.00	LS	\$426,614.00	REMOVAL OF EXISTING BR, STA NO - 00+00 to 00+00	\$426,614.00
215	541-0001	1.00	LS	\$567,125.00	DETOUR BRIDGE - 0015632	\$567,125.00
170	543-9000	1.00	LS	\$1,667,167.00	CONSTRUCTION OF BRIDGE COMPLETE - 0015632	\$1,667,167.00
Bridge 1 Total						\$2,660,906.00

TOTALS FOR CONCEPT NAME 0015632 2-11-22

ITEMS COST:	\$3,670,987.65
TYPICAL SECTION:	\$0.00
AdHoc PRICING:	\$0.00
ESTIMATED COST:	\$3,670,987.65
CONTINGENCY PERCENT:	
ENGINEERING AND INSPECTION:	
ESTIMATED COST WITH CONTINGENCY AND E&I:	

CONFIDENTIALITY NOTICE: This document may contain confidential and/or privileged information. Any unauthorized duplication, disclosure,distribution/retransmission of taking of any action in reliance upon the material in this document is strictly forbidden.

Interoffice Memo

FILE

PI NUMBER	0015632	PROJECT DESCRIPTION	CR 705/BRIDGETOWN ROAD @ SATILLA RIVER 11 MI W OF DOUGLAS
OFFICE	Program Delivery		
DATE	Monday February 21, 2022		

From: Kimberly Nesbitt, State Program Delivery Administrator

To: Erik Rohde, P.E., State Project Review Engineer
via email Mailbox: CostEstimatesandUpdates@dot.ga.gov

Subject: REVISIONS TO PROGRAMMED COSTS

Project Manager:	Raquel Mcmillan
Management Let Date:	1/15/2025
Management Right of Way Date:	12/5/2023

Cost Estimate Review Iteration

Date of Submittal #1	
Date of Submittal #2	
Date of Submittal #3	

Summary of Programmed Costs and Proposed Revised Costs:

Estimate Type	Cost Estimate Amounts (T-Pro Without Inflation)	Last Estimate Date	Revised Cost Estimate
CONSTRUCTION	\$2,700,000.00		\$4,461,866.28
RIGHT OF WAY	\$125,000.00		\$127,000.00
UTILITIES	\$30,000.00		\$0.00

Explanation for Cost Change and Contingency Justification:

The costs are being updated based on conceptual design information. A contingency of 15% has been used based on the values provided in the contingency table in Policy 3A-9 (Cost Estimating Purpose) for Bridge New/Replacement in concept phase.

Attachments:

Detailed Cost Estimate Printout from GDOT 411


Interoffice Memo

Design Phase Leader Validation of Final QC/QA for Construction Cost Estimate Used In This Revision to Programmed Costs:

Consultant Company or GDOT Design Office:	American Consulting Professionals, LLC
-------------------------------------------	----------------------------------------

Printed Name:	Steven Gaines, PE
---------------	-------------------

Title:	Principal/Project Manager
--------	---------------------------

Signature:	
------------	-----------------------------------------------------------------------------------

Date:	2/21/2022
-------	-----------

FOR PROJECTS WITH A LOCAL SPONSOR	
If the project has a local sponsor, the project manager should ensure that the local authority completes the following validation indicating that it has reviewed the construction cost estimate and whether it is in concurrence with the construction costs presented.	
Please select the appropriate validation below upon review of the cost estimate:	
<input type="checkbox"/> I acknowledge that I have reviewed the project construction cost estimate and <u>concur</u> with the costs presented.	
<input type="checkbox"/> I acknowledge that I have reviewed the project construction cost estimate but <u>do not concur</u> with the costs presented.	
Please provide an explanation for non-concurrence.	
Local Authority Name and Title:	
Local Authority Signature:	
Date:	



GEORGIA DEPARTMENT OF TRANSPORTATION
PRELIMINARY ROW COST ESTIMATE SUMMARY

Date: 12/20/21

Project: NA

Revised:

County: COFFEE

PI: 0015632

Description: CR 705/BRIDGETOWN RD @SATILLA RIVER 11 MI W OF DOUGLAS

Project Termini:

Existing ROW: Varies

Parcels: 5

Required ROW: Varies

Land and Improvements \$6,900.00

Proximity Damage \$0.00

Consequential Damage \$0.00

Cost to Cures \$0.00

Trade Fixtures \$0.00

Improvements \$0.00

Valuation Services \$18,750.00

Legal Services \$40,875.00

Relocation \$15,000.00

Demolition \$0.00

Administrative \$45,000.00

TOTAL ESTIMATED COSTS \$126,525.00

TOTAL ESTIMATED COSTS (ROUNDED) \$127,000.00

Prepared By: Cheryl Worthy Pickett

Print Name

Cheryl Worthy Pickett

Signature

12/20/21

Date

Cost Estimation Supervisor : Valencia Carter

Print Name

Valencia Carter

Signature

1/4/2022

Date

NOTE: Supervisor is only attesting that the estimate was completed using the correct information provided for the the project. The Supervisor is not attesting to property values or the accuracy of the market value estimations provided in this report. No Market Appreciation is included in this Preliminary Cost Estimate.

Comments:

Gaines, Steven

From: Westberry, Lisa <lwestberry@dot.ga.gov>
Sent: Friday, October 8, 2021 11:20 AM
To: Jill Brown
Cc: Mcmillan, Raquel; Charlotte Estes
Subject: RE: 0015632 Mitigation Estimates for the Concept Report

Please accept this as my concurrence on the cost estimate provided below.

Thank you,

Lisa Westberry
Special Projects Coordinator



Office of Environmental Services
One Georgia Center, 16th Floor
600 West Peachtree Street, NW
Atlanta, GA, 30308
404.631.1772

From: Jill Brown <jbrown@edwards-pitman.com>
Sent: Friday, October 8, 2021 10:03 AM
To: Westberry, Lisa <lwestberry@dot.ga.gov>
Cc: Mcmillan, Raquel <RMcmillan@dot.ga.gov>; Charlotte Estes <cestes@edwards-pitman.com>
Subject: 0015632 Mitigation Estimates for the Concept Report

Hi Lisa,

Would you please review these PI 0015632 mitigation cost estimates to be included in the Concept Report?

HUC 03070201

Perennial Stream: 120 feet – moderate quality

2018: 90 > 3 sq mi. - not available

Grandfathered: 1080 costs \$30 -85/credit

Wetland: 0.29 acre high quality 0.03 acre moderate quality

2018: 0.31 – not available

Grandfathered: 2.48 costs \$5,000/credit

Mitigation estimates: Stream \$91,800

Wetland \$12,400

Thank you!

Jill Brown | Environmental Project Manager
Edwards-Pitman




Interoffice Memo

FILE

Project No: N/A
County: Coffee
P.I. #: 0015632
Description: CR 705/BRIDGETOWN ROAD @ SATILLA RIVER 11 MI W OF DOUGLAS

Office: Tifton
Date: August 4, 2021

FROM  Shane Pridgen, District Utilities Manager

TO Raquel Mcmillan, Project Manager

SUBJECT PRELIMINARY UTILITY COST ESTIMATE

A review of utilities located on the above referenced project has been conducted without a design concept.. Listed below is a breakdown of the anticipated reimbursable and non-reimbursable cost.

<u>Utility Owner</u>	<u>Reimbursable</u>	<u>Non-Reimbursable</u>	<u>Estimate Based on</u>
Satilla Rural EMC	\$0.00	\$25,000.00	Site Visit / Available Drawings
Windstream	\$0.00	\$7,500.00	Site Visit / Available Drawings
	\$0.00	\$0.00	
	\$0.00	\$0.00	
Total 100.00%	\$ 0.00	\$ 32,500.00	
Department Responsibility 100.00%	\$ 0.00	\$ 0.00	
Local Sponsor Responsibility 0.00%	\$ 0.00	\$ 0.00	PFA Dated N/A with N/A

Update All

** Indicates Potential Utility Aid Request from Local Gov't

Estimate is based on the best available information at the current stage, unforeseen prior rights information may be provided by the Utility Company at a later date that could cause some non-reimbursable costs to shift to the reimbursable cost column.

If additional information is needed, please contact  Kyle Griffin at 229-391-5446.

cc: Steven Gaines, Designer
Nicholas Fields, State Utilities Administrator
Marcella Coll, State Utilities Preconstruction Manager
Jason Willingham, P.E., District Preconstruction Engineer

Attachment 3

Concept Utility Report

Concept Utility Report

Project Number: N/A

District: 4

County: Coffee

Prepared by: Kyle Griffin

PI: 0015632

Date: August 4, 2021

Project Description: CR 705/BRIDGETOWN ROAD @ SATILLA RIVER 11 MI W OF DOUGLAS

The information provided herein has been gathered from Georgia811 and/or field visits and serves as an estimate. Nothing contained in this report is to be used as a substitute for 1st Submission or SUE.

Are SUE services recommended? ☐ Yes ☒ No

Level:

Public Interest Determination (PID): No Use

Is a separate utility funding phase recommended? ☐ Yes ☒ No

Potential Project (Schedule/Budget) Impacts: None known at this time

Capital Improvement Projects (Utilities) Anticipated in the Area: ☐ Yes ☒ No

Project Specific Recommendations for Avoidance/Mitigation: None

Right of Way Coordination:

Environmental Coordination:

Additional Remarks: Utilities are at the back of RW, should be able to construct without utility conflicts.

Concept Utility Report

The following utilities have facilities within the project limits. Utilities have been located using Georgia811 and/or field visits.

Add Row	Del Row	Facility Owner	Facility Owner Contact Email Address	Existing Facilities/Appendances	General Description of Location	Facilities to Avoid (Station/Offset)	Facility Retention Recommended	Comments
+	-	Satilla EMC	Billy Shores, bshores@satillaemc.com	Over Head power	OH Power is 40' East of bridge		<input type="radio"/> Yes <input checked="" type="radio"/> No	
+	-	Windstream	Robbie Boyette, Robbie.Boyette@windstream.com	Unknown wire type and size	UG then OH aerial insert 50' E of bridge at the river		<input type="radio"/> Yes <input checked="" type="radio"/> No	

Attachment 4

Traffic Projections Memorandum

FILE: Coffee County
P.I. # 0015632

DATE: January 7, 2022

FROM: Matt Markham, Deputy Director of Planning

TO: Kimberly Nesbitt, State Program Delivery Administrator
Attention: Raquel McMillan

SUBJECT: **Reviewed** Traffic Data Report for CR 705/Bridgetown Road over Satilla River 11 miles west of Douglas

Per request, we have reviewed the consultant's design traffic forecast for the above project. Based on the information furnished, we find the design traffic forecast to be satisfactory, and the design traffic forecasting task to be complete for the above project. The reviewed and approved design traffic forecast for the above project is shown below.

BRIDGE- ID 069-0035-0

	2021 (Existing Year)	2027 (Base Year)	2029 (Base Year +2)	2047 (Design Year)	2049 (Design Year +2)
AADT	500	500	500	575	575
DHV (AM/PM)	60/60	60/60	60/60	70/70	70/70
K% (AM/PM)	12% / 12%				
D% (AM/PM)	75% / 75%				
24 HR. T% - S.U.	9.5%				
24 HR. T% - COMB.	2.5%				
24 HR. T% - TOTAL	12.0%				
T% - S.U. (AM/PM)	10.5% / 14.5%				
T% - COMB. (AM/PM)	4.0% / 2.5%				
T% - TOTAL (AM/PM)	14.5% / 17.0%				

If you have any questions concerning this information please contact Andre Washington at 404-631-1925.

Keith McCage
HNTB
Design Traffic Consultant to GDOT
404-946-5731

MM/KAM

Attachment 5

S I & A Report

Georgia Department of Transportation Bridge Inventory Data Listing

Processed Date: May-12-2021 17:15 PM

Parameters: Bridge Serial Number

Bridge Serial Number: 069-0035-0

County: Coffee

SUFF. RATING: 49.3

Location & Geography

Structure ID: 069-0035-0

200 Bridge Information: 06

*6 Feature Intersected: SATILLA RIVER

*7A Route Number Carried: CR00705

*7B Facility Carried: BRIDGETOWN RD

9 Location: APP 11 MI W OF DOUGLAS

2 GDOT District: 4841400000 - District Four- Tifton

*91 Inspection Frequency: 24 Date: Oct-31-2019

92A Fracture Critical Insp. Freq: 0 Date: Feb-01-1901

92B Underwater Insp Freq: 12 Date: Nov-23-2020

92C Other Spc. Insp Freq: 0 Date: Feb-01-1901

* 4 Place Code: 00000

*5A Inventory Route(O/U): 1

5B Route Type: 4 - County

5C Service Designation: 1- Mainline

5D Route Number: 00590

5E Directional Suffix: 0. Not applicable

*16 Latitude: 31 - 26.8164

*17 Longitude: 83 - 3.2118

98A Border Bridge: 98B: GA% 00

99 ID Number:

*100 STRAHNET: 0- The Feature is not a STRAHNET route.

12 Base Highway Network: Yes

13A LRS Inventory Route: 692070500

13B Sub Inventory Route: 0

101 Parallel Structure: N. No parallel structure exists

*102 Direction of Traffic: 2- Two Way

*264 Road Inventory Mile Post: 2.07

*208 Inspection Area: Area 04

*104 Highway System: 0- Inventory Route is not on the NHS

*26 Functional Classification: 7- Rural - Major Collector

*204A Federal Route Type: S - Secondary.

*204B Federal Route Number: 00590

105 Federal Lands Highway: 0. Not applicable

*110 Truck Route: 0- The Feature is not part of the National Network for Trucks

217 Benchmark Elevation: 0216.13

* Location ID No: 069-00590F-002.02N

218 Datum: 3- Other

*19 Bypass Length: 6

*20 Toll: 3- On a Free Road or Non-Highway

*21 Maintenance Responsibility: 02-County Highway Agency.

*22 Owner: 02-County Highway Agency.

*31 Design Load: 2- H 15

37 Historical Significance: 5- Not eligible for the National Register of Historic Places

205 Congressional District: 012

27 Year Constructed: 1960

106 Year Reconstructed: 0

33 Bridge Median: 0-None

34 Skew: 0

35 Structure Flared: No

38 Navigation Control: 0- Navigation is not controlled by an Agency

213 Special Steel Design: 0- Not applicable or other

267A Type Paint Super Structure: 3- Epoxy Mastic. Year : 0000

267B Type Paint Sub Structure: 3- Epoxy Mastic Year : 1960

*42A Type of Service On: 1-Highway

*42B Type of Service Under: 5-Waterway

214A Movable Bridge: 0

214B Operator on Duty: 0

203 Type Bridge: E - Steel pile. N. Steel-Concrete O. Concrete O. Concrete

259 Pile Encasement: 1

*43A Structure Type Main material: 1-Concrete

*43B Structure Type Main Type: 4-Tee Beam

45 Number of Main Spans: 10

44 Structure Type Approach: A:0- Other B: 0- Other

46 Number of Approach Spans: 0

226 Bridge Curve: A: Vertical: NoB: Horizontal: No

111 Pier Protection: N - Navigation Control item coded 0, or Feature not a waterway

107 Deck Structure Type: 1 - C-I-P Portland Cement Concrete - Epoxy Coated Rebars

108A Wearing Surface Type: 1. Concrete

108B Membrane Type: 0. None

108C Deck Protection: 8. Unknown

265 Underwater Inspection Area: 2

Signs & Attachments

225 Expansion Joint Type: 02- Open or sealed concrete joint (silicone sealant).

242 Deck Drains: 1- Open Scuppers.

243A Parapet Location: 0- None present.

243B Parapet Height: 0.00

243C Parapet Width: 0.00

238A Curb Height: 1.2

238B Curb Material: 1- Concrete.

239A Handrail Left: 1- Concrete.

239B Handrail Right: 1- Concrete.

*240 Median Barrier Rail: 0- None.

241A Bridge Median Height: 0

241B Bridge Median Width: 0

*230A Guardrail Location Direction Rear: 3- Both sides.

*230B Guardrail Location Direction Fwd: 3- Both sides.

*230C Guardrail Location Opposing Rear: 0- None.

*230D Guardrail Location Opposing Fwd: 0- None.

244 Approach Slab: 3- Forward and Rear.

224 Retaining Wall: 0- None.

233 Posted Speed Limit: 55

236 Warning Sign: No

234 Delineator: No

235 Hazard Boards: Yes

237A Gas: 00- Not Applicable

237B Water: 00- Not Applicable

237C Electric: 00- Not Applicable

237D Telephone: 00- Not Applicable

237E Sewer: 00- Not Applicable

247A Lighting: Street: No

247B Navigation: No

247C Aerial: No

*248 County Continuity No.: 00

36A Bridge Railings: 2- Inspected feature meets acceptable construction date standards.

36B Transition: 2- Inspected feature meets acceptable construction date standards.

36C Approach Guardrail: 2- Inspected feature meets acceptable construction date standards.

36D Approach Guardrail Ends: 2- Inspected feature meets acceptable construction date standards.

Georgia Department of Transportation

Bridge Inventory Data Listing

Processed Date: May-12-2021 17:15:29 PM

Bridge Serial Number: 069-0035-0

County: Coffee

SUFF. RATING: 49.3

Programming Data

201 Project Number: 00000
 202 Plans Available: 4- Plans in Infolmage/GAMS
 249 Proposed Project Number: LOCBR
 250A Reconstruction Approval Status: No
 250B Route Approval Status: No
 250C Approval Status Definition: 0
 250D Approval Status Federal: 0
 251 Project Identification Number: 0015632
 252 Contract Date: Feb-01-1901
 260 Seismic Number: 00000
 75A Type Work Proposed: 0- Not Applicable
 75B Work Done by: 0- Initial Inventory
 94 Bridge Improvement Cost: (X\$1,000) \$1,172
 95 Roadway Improvement Cost: (X\$1,000) \$117
 96 Total Improvement Cost: (X\$1,000) \$1758
 76 Improvement Length: 0'
 97 Year Improvement Cost Based On: 2013
 114 Future AADT: 615
 115 Future AADT Year: 2032

Measurements:

*29 AADT: 410
 *30 AADT Year: 2012
 109 % Truck Traffic: 22
 *28A Lanes On: 2
 *28B Lanes Under: 0
 210A Tracks On: 00
 210B Tracks Under: 0
 *48 Maximum Span Length: 30
 *49 Structure Length: 300
 51 Bridge Roadway Width: 26.4'
 52 Deck Width: 32.3'
 *47 Total Horizontal Clearance: 26.4'
 50A Curb / Sidewalk Width Left: 2
 50B Curb / Sidewalk Width Right: 2
 32 Approach Rdwy. Width: 21'
 *229 Approach Roadway
 Rear Shoulder Left Width: 4.8 Right Width: 5.4 Type: 8 - Grass (Dirt).
 Fwd Shoulder Left Width: 4.7 Right Width: 3.9 Type: 8 - Grass (Dirt).
 Rear Pavement Width: 20.7 Type: 2- Asphalt.
 Forward Pavement Width: 20.9 Type: 2- Asphalt.
 Intersection Rear: 0 Forward: 0

Ratings and Posting

65 Inventory Rating Method: 1-Load Factor (LF)
 63 Operating Rating Method: 1-Load Factor (LF)
 66A Inventory Type: 2 - HS loading.
 66B Inventory Rating: 20
 64A Operating Type: 2 - HS loading.
 64B Operating Rating: 34
 231 Calculated Loads Posting Required
 231A H-Modified: 18 Yes
 231B Type3/Tandem: 18 Yes
 231C Timber: 26 Yes
 231D HS-Modified: 24 No
 231E Type 3S2: 30 No
 231F Piggyback: 00 No
 261 H Inventory Rating: 15
 262 H Operating Rating: 25
 67 Structural Evaluation: 4
 58 Deck Condition: 6 - Satisfactory Condition
 59 Superstructure Condition: 7 - Good Condition
 * 227 Collision Damage:
 60A Substructure Condition: 4 - Poor Condition
 60B Scour Condition: 8 - Very Good Condition
 60C Underwater Condition: 4 - Poor Condition
 71 Waterway Adequacy: 6-Equal to present minimum criteria.
 61 Channel Protection Cond.: 7-Better than present minimum criteria.
 68 Deck Geometry: 5
 69 UnderClr. Horz/Vert: N
 72 Approach Alignment: 7-Between 8 and 6
 62 Culvert: N - Not Applicable
 70 Bridge Posting Required: 4. 0.1 - 9.9% below
 41 Struct Open, Posted, CL: P. Posted for load
 * 103 Temporary Structure: No
 232 Posted Loads
 232A H-Modified: 18
 232B Type3/Tandem: 18
 232C Timber: 26
 232D HS-Modified: 00
 232E Type 3s2: 00
 232F Piggyback: 00
 253 Notification Date: Feb-01-1901
 258 Federal Notify Date: Feb-01-1901

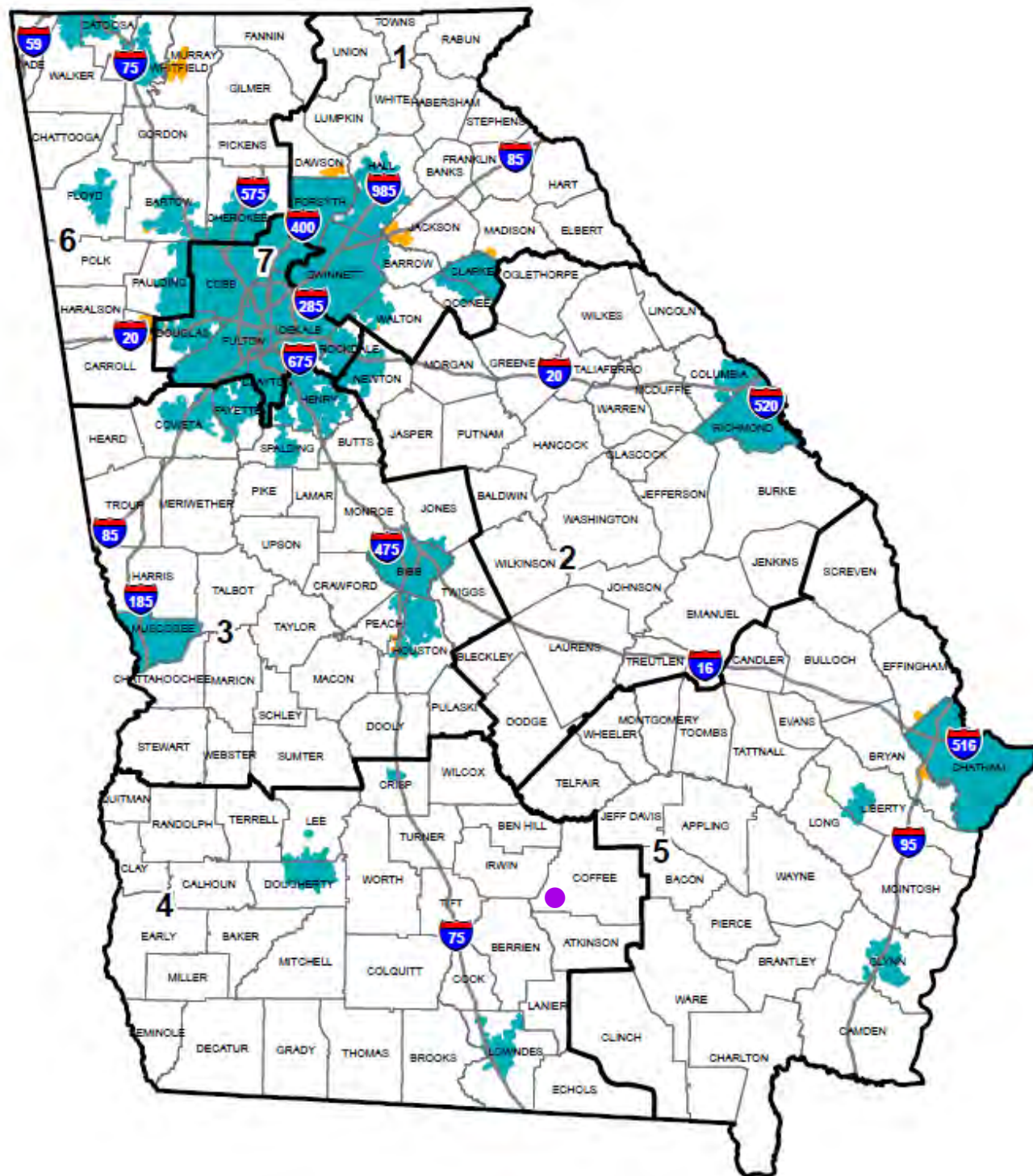
Hydraulic Data

113 Scour Critical: U.Unknown Foundation; not evaluated for scour.
 216A Water Depth: 02.2
 216B Bridge Height: 19.0
 222 Slope Protection: 1
 221A Spur Dike Rear:
 221B Spur Dike Fwd:
 219 Fender System: 0- None.
 220 Dolphin:
 223A Culvert Cover: 000
 223B Culvert Type: 0- Not Applicable
 223C Number of Barrels: 0
 223D Barrel Width: 0
 223E Barrel Height: 0
 223F Culvert Length: 0
 223G Culvert Apron: 0
 39 Navigation Vertical Clearance: 0'
 40 Navigation Horizontal Clearance: 0
 116 Navigation Vertical Clear Closed: 0

53 Minimum Vertical Clearance Over Rd: 99' 99"
 54A Under Reference Feature: N- Feature not a highway or railroad.
 54B Minimum Clearance Under: 0' 0"
 *228 Minimum Vertical Clearance
 228A Actual Odometer Direction: 99'99"
 228B Actual Opposing Direction: 99'99"
 228C Posted Odometer Direction: 00'00"
 228D Posted Opposing Direction: 00'00"
 55A Lateral Underclearance Reference: N- Feature not a highway or railroad.
 55B Lateral Underclearance on Right: 0
 56 Lateral Underclearance on Left: 0
 10A Direction of Travel for Max Min: 0
 10B Max Min Vertical Clearance: 99'99"
 245A Deck Thickness Main: 6.0
 245B Deck Thickness Approach: 0
 246 Overlay Thickness: 0

Attachment 6

MS4 Concept Report Summary



LEGEND

- Project Location
- GDOT MS4 Permitted Area (2012)
- GDOT MS4 Permitted Area (2017)

Georgia DOT

MS4 Project Level Exclusion
PI 0015632
Coffee County



FIGURE
1

Attachment 7

Minutes – Concept Team Meeting

MEETING MINUTES



Meeting Date: 11-3-21 **Date Issued:** 11-19-21
Location: Microsoft Teams
Project Name: CR 705/Bridgetown Road @ Satilla River 11 Miles W of Douglas (PI 0015632) Coffee County
Purpose: Concept Team Meeting
Notes by: Steven Gaines, PE **American Project #:** 520GA056

Meeting Attendees		
Name	Company	Email
Raquel McMillan	GDOT/AECOM	rmcmillan@dot.ga.gov
Bryan Williams	GDOT/AECOM	brywilliams@dot.ga.gov
Katelyn Reed	GDOT/OES	kreed@got.ga.gov
Oladimeji Onabanjo	GDOT/Office of Traffic Operations	oonabanjo@dot.ga.gov
Justin Willingham	GDOT/D4 Traffic Operations	juwillingham@dot.ga.gov
Jason Willingham	GDOT/D4 Preconstruction	jwillingham@dot.ga.gov
Donn Digamon	GDOT/Bridge	dodigamon@dot.ga.gov
Shane Pridgen	GDOT/D4 Utilities	spridgen@dot.ga.gov
Robert Binns	GDOT/Transportation Data	rbinns@dot.ga.gov
Kyle Griffin	GDOT/D4 Utilities	kgriffin@dot.ga.gov
Brittany Gillis	GDOT/D4 Utilities	bgillis@dot.ga.gov
Dennis Carter	GDOT/D4 Planning	dcarter@dot.ga.gov
Jill Brown	Edwards-Pitman	jbrown@edwards-pitman.com
Draper Suttles	Edwards-Pitman	dsuttles@edwards-pitman.com
Dania Aponte	Edwards-Pitman	daponte@edwards-pitman.com
Steve Carver	Coffee County EMS	steve.carver@coffeecounty-ga.gov
Billy Shores	Satilla EMC	bshores@satillaemc.com
Janice Bishop	Windstream	janice.bishop@windstream.com
Robert Benson	American Consulting Professionals	rbenson@acp-ga.com
Howard Anderson	American Consulting Professionals	handerson@acp-ga.com
Allen Peterfreund	American Consulting Professionals	allen.peterfreund@acp-ga.com
Kaitlyn Diehsner	American Consulting Professionals	kdiehsner@acp-ga.com
Steven Gaines	American Consulting Professionals	sgaines@acp-ga.com

Welcome and Introductions

- Raquel McMillan from GDOT Office of Program Delivery (GDOT OPD) started with a brief introduction on how the meeting would run. This virtual concept team meeting involved CR 705/Bridgetown Road @ Satilla River 11 Miles West of Douglas (PI 0015632). Next, Steven Gaines with American Consulting Professionals (ACP) talked through the key points of the concept report via power point presentation for the project. During the meeting, Allen Peterfreund (ACP) and staff from GDOT Offices participated in discussion at various points in the presentation.

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Project Background

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| Discussion | <ul style="list-style-type: none"> Project Background –This project proposes to replace the existing culvert (structure ID 069-0035-0) on CR 705/Bridgetown Road <ul style="list-style-type: none"> ROW Authorization: December 2023 Let Date: January 2025 Open to Traffic: November 2025 No comments |
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Project Justification

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| Discussion | <ul style="list-style-type: none"> Bridge Posting:
H-Modified (18 T)
Type3/Tandem (18T)
Timber (26 T) Cracking throughout and spalling under span 1 deck Minor flexural cracking in all beams Minor cracking and spalling in caps Moderate corrosion and paint loss on all piles No comments |
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Existing Conditions

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| Discussion | <ul style="list-style-type: none"> Bridge – Built in 1960, 10 spans (concrete T-beams) on concrete caps with partially encased steel H-piles. 300' total length with 32.3' deck width Roadway – Major Collector, 2 – 11 ft lanes with variable width shoulders Adjacent Projects – 0016241 (SR 149 from Atkinson County Line to SR 32 – TIA) No comments |
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Traffic Data

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| Discussion | <ul style="list-style-type: none"> AADT (15% Trucks)
 Current Year (2020) - 500
 Open Year (2025) – 500
 Design Year (2045) – 575 No comments |
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Environmental Coordination

	<ul style="list-style-type: none"> • NEPA - CE anticipated, A3M scheduled for August 2022 • Ecology – Section 7 consultation expected (Eastern Indigo Snake/Gopher Tortoise & Atlantic Sturgeon/Critical Habitat • History - One potentially eligible resource • Archeology - Anticipate ESAs will be outside of project footprint • Public Involvement – Virtual PDOH • Air Quality - CO Hotspot Analysis Not Required • Noise - Anticipate Type III with no modeling
Discussion	<ul style="list-style-type: none"> • No Comments

Design Features

	<ul style="list-style-type: none"> • Roadway – 2-11 foot lanes, 55 mph Speed Design, 6% emax, 6' shoulder (4' paved) • Bridge – 360' length, 33.08' width
Discussion	<ul style="list-style-type: none"> • No Comments

Alternatives Comparison

	<ul style="list-style-type: none"> • Preferred Alternative - Replace Bridge on Existing Alignment, Offsite Detour, 1000 If Roadway Approach Reconstruction, Lower Construction & Right-of-Way Cost, Less Community and Environmental Impacts • Offsite Detour Coordination Responses: EMS – Major Concerns for Increased Response Times (Bridgetown Volunteer Fire Department), County Administration – Support/No Comments, Schools – Pending • Alternative 2 - Maintain Traffic on Existing Alignment, Additional 800 If Approach Reconstruction, Higher Construction & Right-of-Way Costs, Greater Property and Environmental Impacts • No Build Alternative – The bridge needs to be replaced because it does not meet current design standards.
Discussion	<ul style="list-style-type: none"> • Steve Carver (Coffee County EMS) shared the following information and concerns about the implementation of an off-site detour for the project. The Bridgetown Volunteer Fire Department is located approximately 1 miles south of the project. The closing of the road would cause delays for personnel driving to the station and delays for fire trucks departing from the station. Some of the county roads that would be used in lieu of the CR 705/ Bridgetown Road become nearly impassible during major rain events. The nearest fire station is located as CR 158 @ CR 47. Scott Carver commented that the maximum standard response time is 12 minutes.

Project Cost Estimate

		PE Funding	Section 404 Mitigation	ROW	Reimbursable Utilities	Construction	Total Cost
	Program Cost	\$905,000	N/A	\$125,000	\$30,000	\$2,700,000	\$3,730,000
	Estimated Cost	\$905,000	\$104,200	\$125,000*	\$0	\$3,314,731	\$4,448,931
Discussion	<ul style="list-style-type: none"> ROW cost estimates have been submitted for approval 						

Questions / Additional Discussion

Discussion	<ul style="list-style-type: none"> An onsite detour alternative needs to be developed for comparison with the permanent offset alignment since the impacts of the offsite detour on EMS response times is significant Donn Digamon commented that bridge coordination for the Atlantic Sturgeon can be a very long and complicated process. Coordination with NMFS requires a longer time period and may extend the schedule for the project Steve Carver commented that upstream flooding has occurred in several areas where GDOT has replaced bridge (441N – Riverbend Estates/221 N/SR 135 South – Old Pierson Highway).
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Action items	Person responsible	Deadline
Develop Onsite Detour Alternative	Steven Gaines	12/10/21
Submit Meeting Comments	All	11/10/21
Revise Preferred Alternative in Concept Report	Steven Gaines	12/17/21